

**VALUE ENGINEERING CHANGE PROPOSAL
MISSOURI DEPARTMENT OF TRANSPORTATION**

Conceptual Proposal **Final Proposal** Date 4/11/2012

Contract ID 120120-GO8 Job No. J7P0659

County Bates Route 71 Original Contract Amount 5,495,166.74

Contractor Lehman Construction LLC By Adam Carroll

Designed By MODOT Phone 573-796-8101

VECP# 12-27 (to be completed by C.O.) VECP or PDVECP

1. Description of existing requirements and proposed change(s). Advantages/Disadvantages

Current requirements for raising and regrading East Service Rd at Sta. 1315+00 to 1333+00 and at Sta. 1359+00 to 1414+23 consist of reshaping and disturbing more area than we feel is necessary. Currently there is an existing 20' wide road bed with sound durable subgrade in between the above stations. We feel rather than reshaping slopes, raising elevation, and restoring seed growth in these areas, we would propose to grade existing subgrade to plan slopes and place 8" of gravel or crushed stone B. This would eliminate Class A (6987CY), embankment in place (23350 CY), class 3 excavation (4 CY), rock lining (1.8 CY), 18" GrnB nine (14 LF) w/ FES (2), seeding (6.75 AC).

2. Estimate of reduction in construction costs. \$83,531.55

3. Prediction of any effects the proposed change(s) will have on other department costs, such as maintenance and operations.
none

4. Anticipated date for submittal of detailed change(s) of items required by Section 104.6 of the Specifications.

4/12/12
(date)

5. Deadline for issuing a change order to obtain maximum cost reduction, noting the effect of contract completion time or delivery schedule.

5/1/2012 _____
(date) (effect)

6. Dates of any previous or concurrent submission of the same proposal.

(date and/or dates)

Additional Comments:

See Attached Breakdown for detailed cost breakdown.

**** Portion Below This Line To Be Filled Out by MoDOT ****

Comments:
I recommend acceptance of this proposal. The planned change in elevation in this region is less than 12 inches and the existing roadbed has served the current residents adequately. By placing the new aggregate surface over the existing roadbed we will maintain the current structure in place and minimize the land disturbance and time required to complete this section of roadway.


Submitted By Resident Engineer

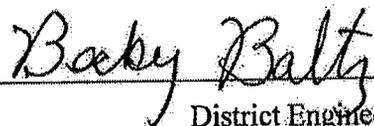
Digitally signed by Randall E. Aulbur, P.E.
DN: cn=Randall E. Aulbur, P.E, o=MoDOT 5W, ou=Construction and Materials,
email=randal@aulbur@modot.mo.gov, c=US
Date: 2012.04.19 08:44:27 -05'00'

_____ Date

Comments: I concur

Approval Recommended

Rejection Recommended


District Engineer

5-2-12
Date

Comments: I concur as 75/25.

Approval Recommended

Rejection Recommended

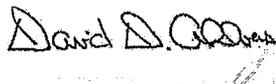

Federal Highway Administration
Required for FHWA Full Oversight Projects

05-21-2012
Date

Comments: Approved per District recommendation as a Practical Design Value Engineering at 75/25 split.

Approval

Rejection


State Construction and Materials Engineer

Digitally signed by Dennis Bryant
DN: cn=Dennis Bryant, o=MoDOT,
ou=3H35,
email=dennis.bryant@modot.mo.gov,
c=US
Date: 2012.05.22 07:57:56 -05'00'

_____ Date

Distribution: Resident Engineer, Project Manager, District Construction & Materials Engineer, State Construction & Materials Engineer, FHWA Value Engineering Administrator -- MoDOT, P. O. Box 270, Jefferson City, MO 65102