

Additional Comments:

**** Portion Below This Line To Be Filled Out by MoDOT ****

Comments: This VE will provide proper material for our acceleration/deceleration extensions as well as save on costs by not removing material that is not necessary to be removed along I-44. All slopes left in place by this change will be on a 4:1 slope or flatter. Based on our review, we recommend approval of this VE submittal.



Digitally signed by Gregory R. Chapman, P.E.
DN: cn=Gregory R. Chapman, P.E., o=MoDOT, ou=Joplin
Construction, email=gregory.r.chapman@modot.mo.gov,
c=US
Date: 2012.09.26 10:33:41 -05'00'

9/26/12

Submitted By Resident Engineer

Date

Comments:

I concur.



Digitally signed by Marilyn Gabriel
DN: cn=Marilyn Gabriel, o=MoDOT, ou=Construction and
Materials, email=Marilyn.Gabriel@modot.mo.gov, c=US
Date: 2012.09.28 07:56:51 -05'00'

Approval
Recommended

Rejection
Recommended

District Engineer

Date

Comments:

FHWA received this VECP on 10/01/12 for our review and approval. Share 50/50



Digitally signed by Felix R. Gonzalez
DN: cn=Felix R. Gonzalez, o, ou,
email=felix.r.gonzalez@dot.gov, c=US
Date: 2012.10.05 10:09:42 -05'00'

10/05/2012

Approval
Recommended

Rejection
Recommended

Federal Highway Administration
Required for FHWA Full Oversight Projects

Date

Comments:

Approved per District recommendation as a VECP at 50/50 split



Digitally signed by Dennis Bryant
DN: cn=Dennis Bryant, o=MoDOT, ou=3H35,
email=dennis.bryant@modot.mo.gov, c=US
Date: 2012.10.09 08:24:15 -05'00'

Approval

Rejection

State Construction and Materials Engineer

Date

Distribution: Resident Engineer, Project Manager, District Construction & Materials Engineer, State Construction & Materials Engineer, FHWA Value Engineering Administrator – MoDOT, P. O. Box 270, Jefferson City, MO 65102

Newton Cty J710691

Class A Excavation Elimination at Connection of Loop Ramps & I-44

VE Class A Excavations Savings

| | X | Class A Excavation - Loop Ramps | 8,376.0 | CYS | X | \$ | 6.50 | /CY | \$ | 54,444.00 |
|----|---|--|---------|-----|---|----|----------|-----|----|-------------|
| -1 | X | Class A Excavation - WB Accel & EB Decel | 3,223.0 | CYS | X | \$ | 6.50 | /CY | \$ | (20,949.50) |
| -1 | X | Seeding Cool Season Mixtures | 1.3 | AC | X | \$ | 1,450.00 | /EA | \$ | (1,845.29) |

SUBTOTAL = \$ 31,649.21

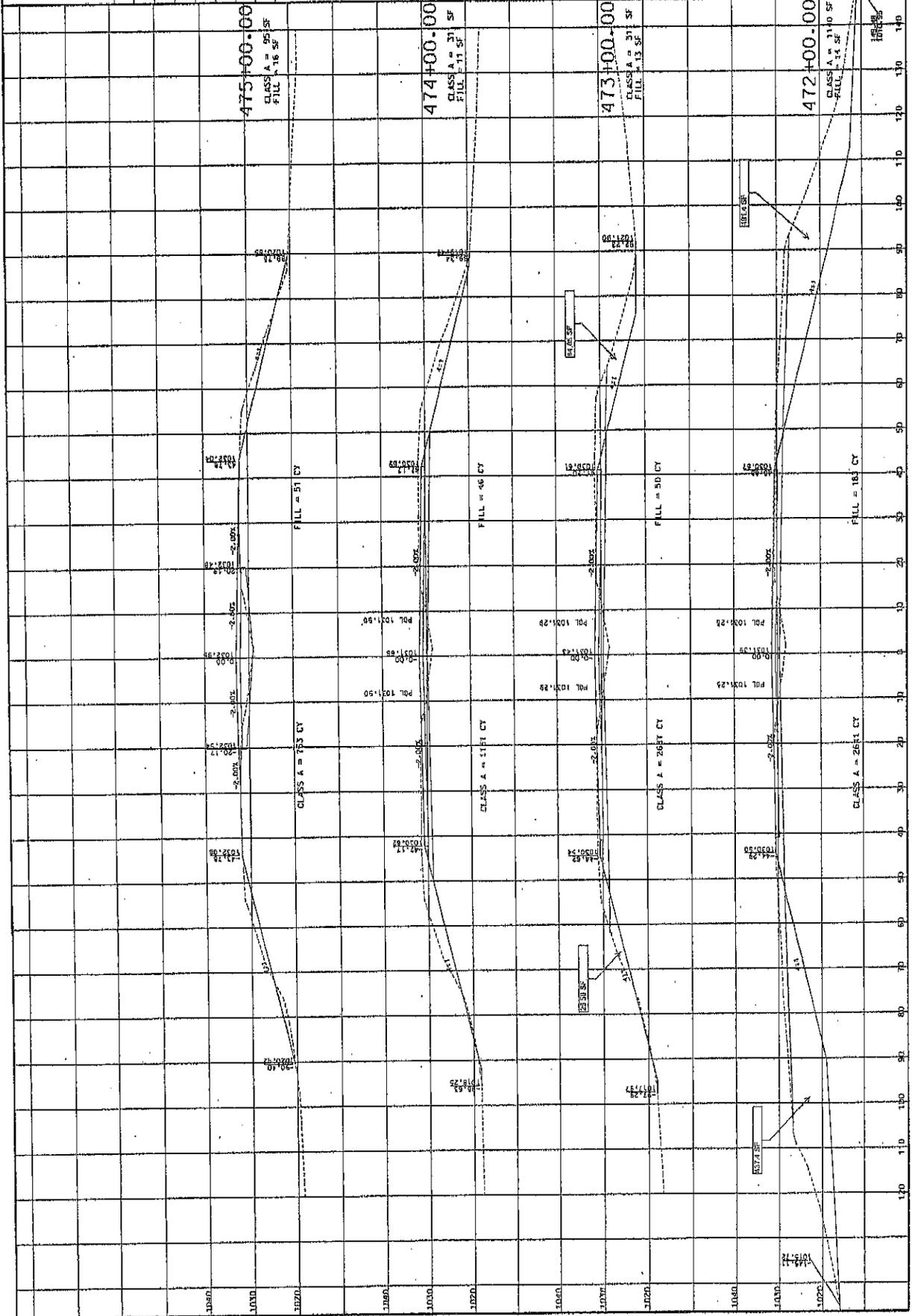
GROSS SECTION SHEET
 1-44 EAST OF BUS. 49
 SHEET 2 OF 6



MISSOURI HIGHWAYS AND TRANSPORTATION
 COMMISSION
 103 WEST CAPITOL
 2152 S. WASHINGTON ST., 4TH FLOOR
 64601-1000
 1-888-454-MDOTT (1-888-454-6262)

DATE PREPARED: 2/7/2012
 DRAWN BY: JMD
 SHEET NO.: 1-44
 COUNTY: SM
 PROJECT NO.: 20
 DIVISION: NEWTON
 PROJECT NAME: JACOBS
 CONTRACT NO.:
 SURVEY NO.:
 DATE: 1/11/12

IF A SERIAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRONICALLY SEALED AND DATED.



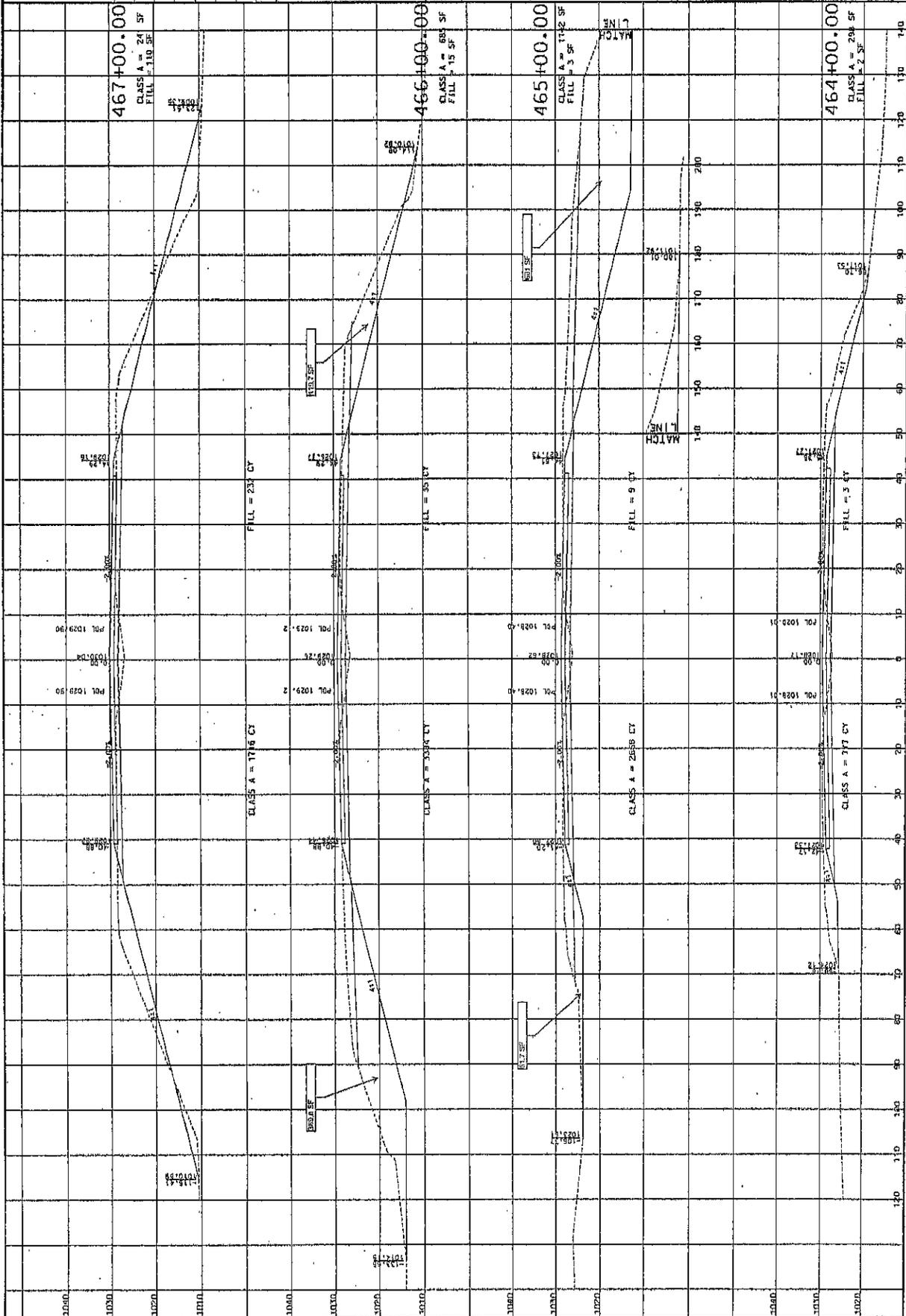
CROSS SECTION SHEET
 1-44 WEST OF BUS. 49
 SHEET 5 OF 6



103 WEST CAPITOL
 JEFFERSON CITY, MO 65102
 1-888-454-4000 (1-888-275-8282)

IF A SEAL IS PRESENT ON THIS SHEET IT HAS BEEN ELECTRICALLY SEALED AND DATED.

| | |
|---------------|-----------|
| PROJECT NO. | |
| BRIDGE NO. | |
| DESCRIPTION | |
| DATE | |
| CONTRACT NO. | JT10691 |
| CONTRACT TIT. | |
| COUNTY | NEWTON |
| SHEET NO. | 57 |
| TOTAL SHEETS | 77 |
| DATE | 2/16/2012 |
| SCALE | 1"=4' |
| MO | MO |
| PROJECT | 1-44 |
| CLASS | CLASS A |
| FILL | FILL |



THIS SHEET SHOULD NOT BE REPRODUCED OR COPIED WITHOUT THE WRITTEN PERMISSION OF THE MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION.



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2602 NORTH STADIUM BLVD
COLUMBIA, MO 65202

WWW.EMERYSAPP.COM

September 14, 2012

Mr. Greg Chapman, R.E.
Missouri Dept. of Transportation
1501 Malcolm Mosby Dr.
Neosho, MO 64850

**RE: Value Engineering Class A Excavation
Rt I-44 Newton Cty
Job J710691**

Mr. Chapman:

This letter is being submitted in regards to the attached Value Engineering Proposal for the elimination of Class A Excavation at the tie in of each of the Existing Clover Leaf Ramps. This proposal also includes modifications to the Class A Excavation shown in Stage 1. Each of these grading adjustments will benefit the overall project in the following ways.

First, we are requesting to eliminate a large portion of the Class A Excavation along I-44 between Stations 465+00 & 466+00 as well as between 472+00 & 473+00. The cross sections in these areas call for the removal of each of the Existing Clover Leaf Tie Ins. We intend to completely remove the existing pavement as specified in the removals. Once the pavement is removed we will grade the remaining Class A material as shown on the attached red lined cross sections. This design modification will eliminate approximately 8,376 cy of unnecessary Class A Excavation. If this elimination is approved we will use the Right of Way leading up to these station ranges for a waste area during the construction of the new ramps. Any waste materials placed adjacent to the existing clover leaf ramps will be shaped with the intentions of achieving a 4:1 slope or flatter.

The second design modification we are requesting includes a resolution to a Stage 1 design conflict with the Class A Excavation. Stage 1 consists of the construction of each of the Accel and Decel lanes on I-44 as well as the temporary connections on Range Line Road. In order to complete the grading in each of these areas we were required to obtain fill from other areas of the project. All available excavation quantities outside of the existing roadway limits consisted of very shallow cuts that would have consisted primarily of sod and light vegetation. In order to obtain suitable material for the construction of the roadway embankments in Stage 1 we requested to be allowed to borrow from

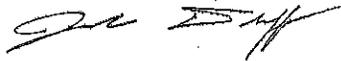
MoDOT Right of Way adjacent to the new fill areas. The total borrow we are requesting to resolve the design conflict and expedite the grading in Stage 1 is approximately 3,223 cy.

In conclusion each of the grading modifications listed above will benefit the overall project in various ways. The elimination of Class A Excavation adjacent to I-44 will provide more traversable recovery zones for the traveling public and the implementation of the onsite borrow allowed for a more suitable fill material to be used in the construction of new Roadway Embankments. In addition to Improvements in safety and quality these design enhancements will also save time during Stage 1 Construction while reducing the overall contract costs.

Please review the attached proposal and let me know if you have any questions.

Sincerely,

Emery Sapp & Sons, Inc.



Josh Doerhoff
Project Manager

Digitally signed by Josh Doerhoff
DN: cn=Josh Doerhoff, o=Emery
Sapp & Sons Inc, ou,
email=josh.doerhoff@emerysapp.c
om, c=US
Date: 2012.09.14 14:58:45 -05'00'

